

STATE OF OHIO DEPARTMENT OF HIGHWAYS W00-281-(1.95) (12.88)

FED. RD. DIVISION	STATE	PROJECT	TYPE FUNDS
2	OHIO	STATE	

W00-281-(1.95)(12.88)

WOOD COUNTY LIBERTY & PORTAGE TOWNSHIPS

CONVENTIONAL SIGNS

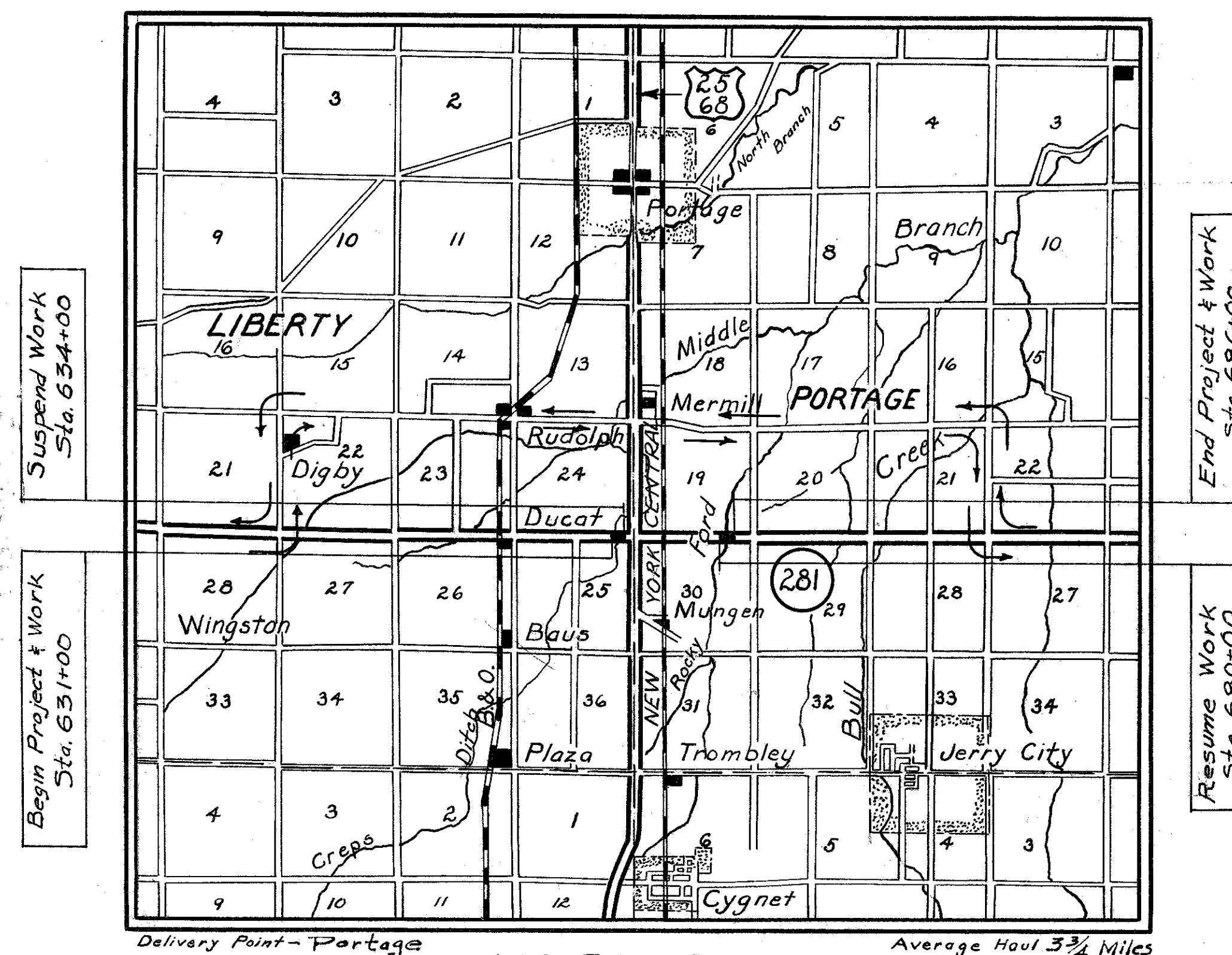
County Line	---
Township Line	---
City or Village Line	---
Center Line	---
Property Line	---
Fence Line	---
Pole Line	---
Railroad	---
Guard Rail	---

INDEX of SHEET

Title Sheet	1
Typical Section	2
General Notes, Subsummaries, pavement computation, and General Summaries	3
Plan and Profile Sheets	4, 10
Cross Section Sheets	5, 11
Channel Cross Sections	6, 12
Structures over 20 Ft. span	7-9 & 13-15

LINE DATA

Begin Project & Work	Sta. 631+00
Suspend Work	Sta. 634+00
Gross length = Net length	= 300 Lin. Ft.
Resume Work	Sta. 680+00
End Work & Project	Sta. 686+00
Gross length = Net length	= 600 Lin. Ft.
Total net length	= 300 + 600 = 900 Lin. Ft. = 0.170 Miles



LOCATION PLAN

Scale in Miles
Portion to be removed
State Highways
Other Roads
Detours

SCALES

Plan	1" = 20'
Profile (Vert.)	1" = 20'
Profile (Hor.)	1" = 20'
Cross Sections	1" = 5'

STANDARD CONSTRUCTION DRWGS

G 7-07	1-2-53
R 7-1	6-1-53
L-3	4-1-50
L-3A	4-1-50
I-12, 3, 4, 5	2-20-45
A-5-1-54	12-1-54
I-15, No. 1	12-1-54
I-15, No. 2	12-1-54
C-9-1-54	12-1-54
A-1-54	12-1-54

SUPPLEMENTAL SPECIFICATIONS

B-119	Rev. 12-1-54
L-209, 12	7-17-54

The Standard Specifications of the State of Ohio, Department of Highways, including changes and Supplemental Specifications listed in the proposal shall govern this improvement.

I hereby approve these plans and declare that the making of this improvement will require the closing of the highway to traffic and that detours will be provided as indicated on the plans.

The right-of-way for this improvement will be provided by the State of Ohio.

Approved L. H. Prew
Date Jan. 18, 1955 Division Deputy Director

Approved John J. Deery
Date 2-16-55 Deputy Director of Planning & Programming

Approved Richard Ornt
Date 2-9-55 Engineer of Bridges

Approved K. H. Roth
Date 2-11-55 Engineer of Location & Design

Approved J. F. Gurd
Date 2-11-55 Deputy Director of Design & Construction

Approved P. F. Gaudin
Date 2-16-55 First Assistant Director

Approved J. C. Gaudin
Date 2-16-55 Director of Highways

CONSTRUCTION
BUREAU
FEB 18 1955
GROUND PHOTOGRAPH

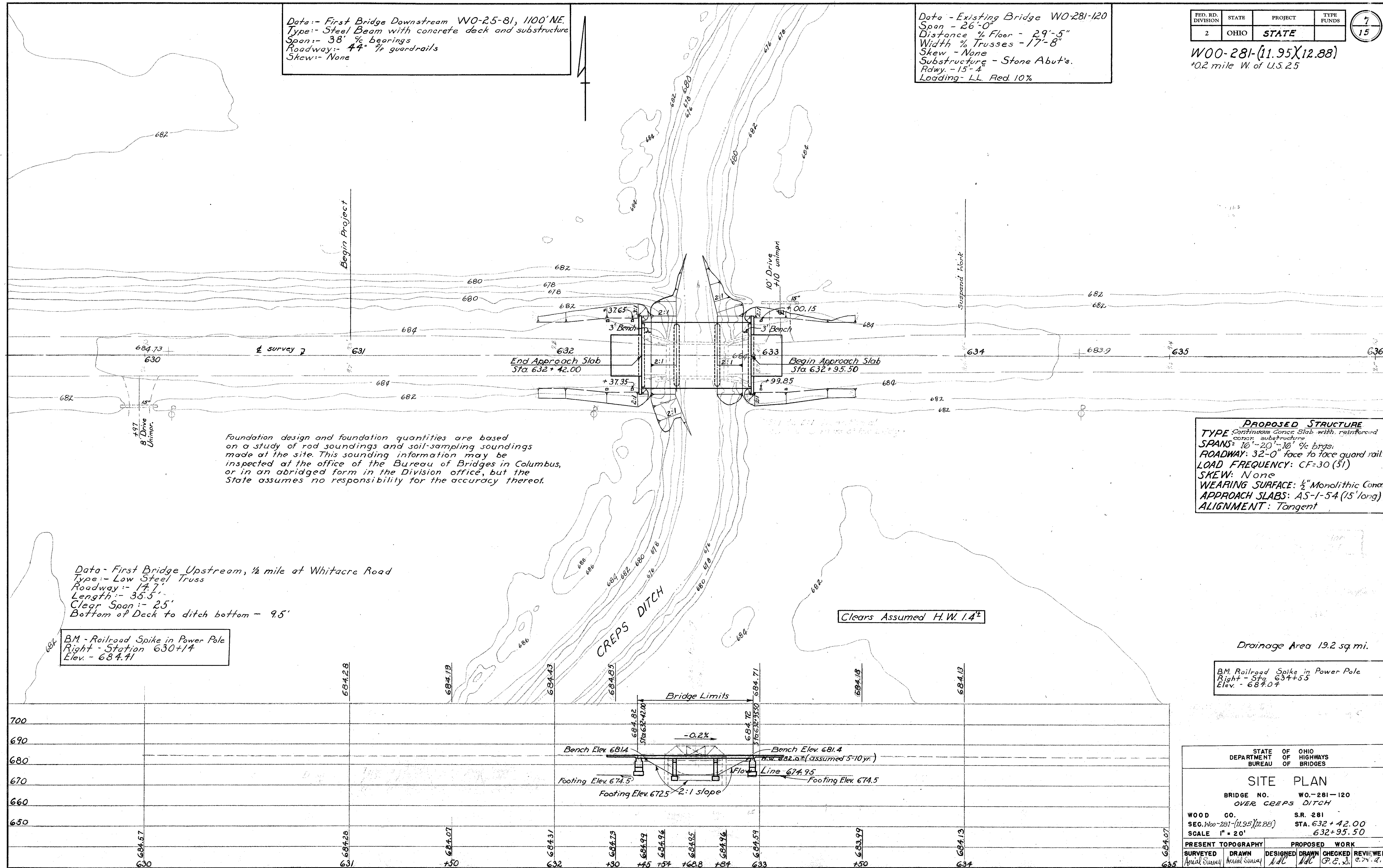
Data:- First Bridge Downstream WO-25-81, 1100' NE.
 Type:- Steel Beam with concrete deck and substructure
 Span:- 38' % bearings
 Roadway:- 44' % guardrails
 Skew:- None

Data - Existing Bridge WO-281-120
 Span - 26'-0"
 Distance % Floor - 29'-5"
 Width % Trusses - 17'-8"
 Skew - None
 Substructure - Stone Abut's.
 Rdwy. - 15'-4"
 Loading - LL. Red. 10%

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7
15

W00-281-(11.95)(12.88)
 10.2 mile W. of U.S. 25



PROPOSED STRUCTURE
 TYPE: Continuous Concr. Slab with reinforced concr. substructure
 SPANS: 10'-20'-10' % brgs.
 ROADWAY: 32'-0" face to face guard rail
 LOAD FREQUENCY: CF=30 (51)
 SKEW: None
 WEARING SURFACE: 1/2" Monolithic Concr.
 APPROACH SLABS: AS-1-5'4 (15' long)
 ALIGNMENT: Tangent

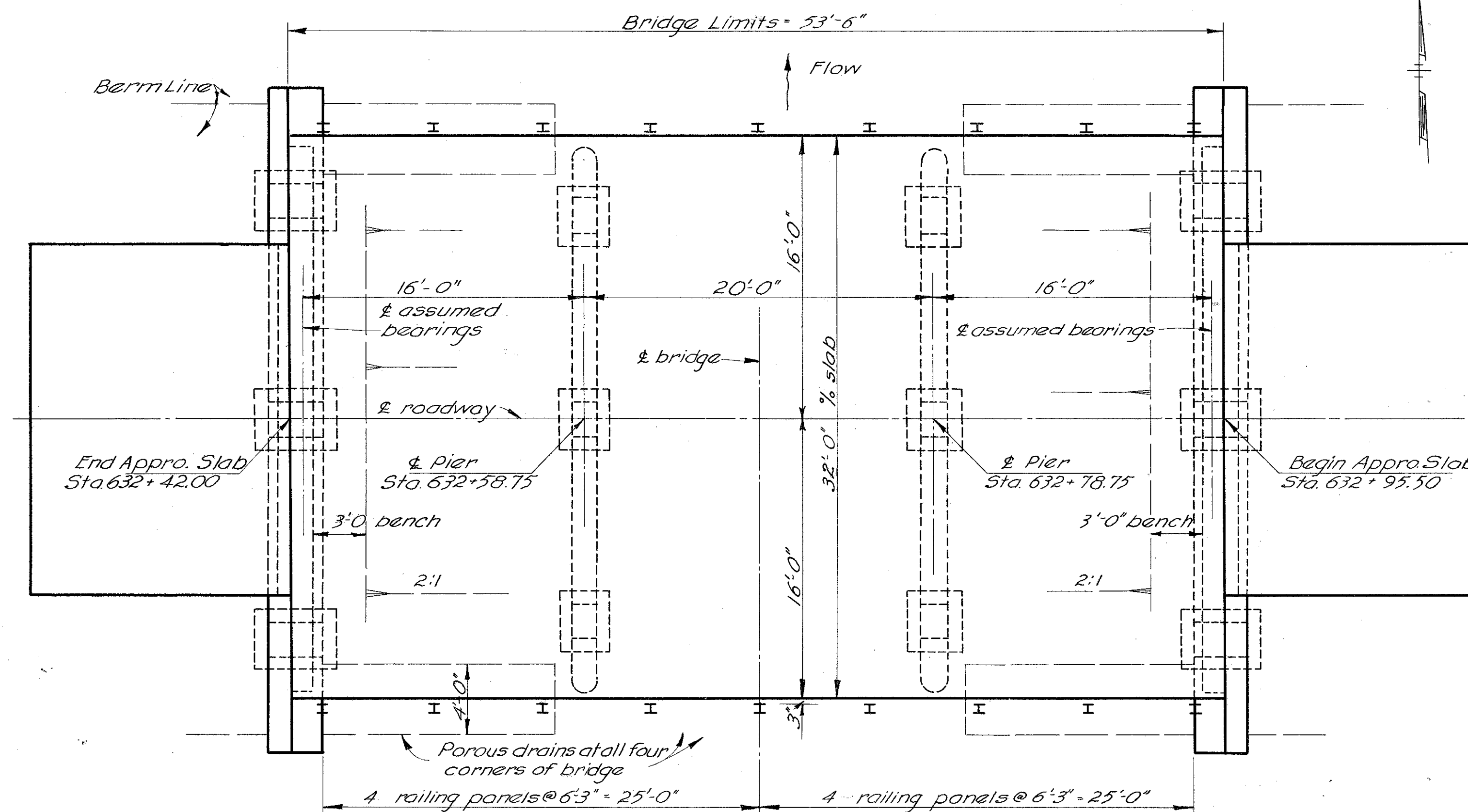
Drainage Area 19.2 sq. mi.

BM. Railroad Spike in Power Pole
 Right - Sta. 634+55
 Elev. - 684.04

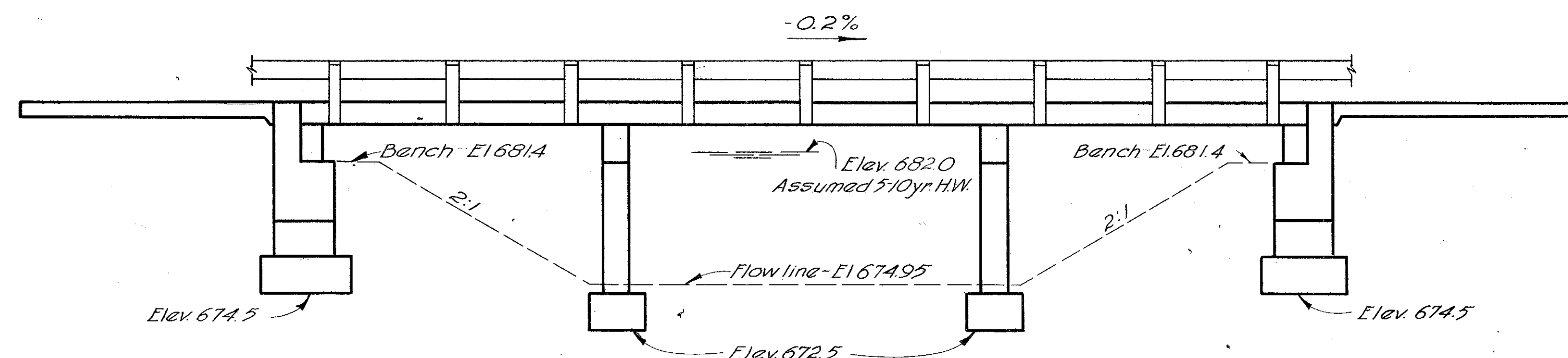
STATE OF OHIO DEPARTMENT OF HIGHWAYS BUREAU OF BRIDGES	
SITE PLAN	
BRIDGE NO. WO-281-120	OVER CREPS DITCH
WOOD CO. S.R. 281	STA. 632+42.00
SEC. W00-281-(11.95)(12.88)	632+95.50
SCALE 1" = 20'	
PRESENT TOPOGRAPHY	PROPOSED WORK
SURVEYED	DESIGNED
DRAWN	CHECKED
APPROVED	REVIEWED

BFG 12-23-54

Woo-281-(11.95)(12.88)



GENERAL PLAN



GENERAL ELEVATION

ESTIMATED QUANTITIES						
Item	Total	Unit	Description	Super	Abut.	Pier
E-2	Lump	Sum	Cofferdams, cribs & sheeting			
E-2	135	Cu.Yd.	Unclassified excavation		135	
E-2	11	Cu.Yd.	Rock excavation		3	8
E-3	125	Cu.Yd.	Channel excavation			
S-1	60	Cu.Yd.	Class "C" concrete, superstructure & pier caps	52		8
S-1	47	Cu.Yd.	Class "E" concr. piers & abuts. above footings		41	6
S-1	12	Cu.Yd.	Class "E" concrete, footing		7	5
S-4	21,539	Lbs.	Reinforcing steel	12,758	5,057	3,621
S-14	107	Lin. Ft.	Railing (Type I-15.13 with galvanized steel posts)	107		
S-24	Lump	Sum	Removal of existing structure			
S-29	12	Cu.Yd.	Porous backfill		12	
S-29	11	Cu.Yd.	Porous drains on embankment slopes			11

REINFORCING STEEL LIST					BENDING DIAGRAM					PIERS				
MARK	NO.	LENGTH	WEIGHT	SHA						MARK	NO.	LENGTH	WEIGHT	SHA
ABUTMENTS														
R1001	8	30'-6"	1090	S						P901	48	11'-10"	1931	B
R901	48	7'-1"	1154	B						P701	8	31'-6"	515	S
R801	8	37'-6"	801	S						P601	8	29'-6"	355	B
R501	8	8'-6"	71	S						P501	4	29'-6"	123	S
R502	16	5'-0"	84	S						P502	8	5'-0"	42	B
R503	24	4'-6"	113	S						P503	72	4'-2"	314	B
R504	4	30'-6"	127	S						P401	64	8'-0"	341	B
R505	8	37'-6"	314	S						SUPERSTRUCTURE				
R506	16	6'-7"	110	B						A700	81	19'-3"	3187	S
R507	16	8'-4"	139	B						B700	26	15'-0"	797	B
R508	26	8'-0"	217	B						C700	26	13'-4"	709	B
R509	104	6'-9"	733	B						D700	13	14'-8"	390	S
R510	24	5'-9"	144	B						E700	13	10'-8"	283	S
REPLACEMENT BARS										F700	62	12'-6"	1584	S
RE10	1	7'-3"	31	S						G700	30	7'-0"	429	S
RE9	1	6'-10"	23	S						H700	30	5'-0"	307	S
RE8	1	6'-6"	17	S						J601	32	12'-0"	577	S
RE7	1	6'-3"	13	S						K601	16	11'-10"	284	S
RE6	1	5'-11"	9	S						M601	48	31'-6"	2271	S
RE5	1	5'-7"	6	S						N601	41	31'-6"	1940	S
RE4	1	5'-3"	4	S										

GENERAL NOTES

REFERENCE shall be made to Std. Drwg. CS-1-54 revised 12-1-54.

REMOVAL OF EXISTING STRUCTURE: When no longer needed to maintain traffic the existing structure shall be removed. Structural steel shall be carefully dismantled and piled along the right-of-way for disposal by the State's forces. The remainder of the removed materials shall become the property of the Contractor. The substructure shall be removed to finished ground line or where necessary for new construction.

FOOTINGS shall extend to minimum of 3" into solid rock, or to the elevation shown, whichever is lower.

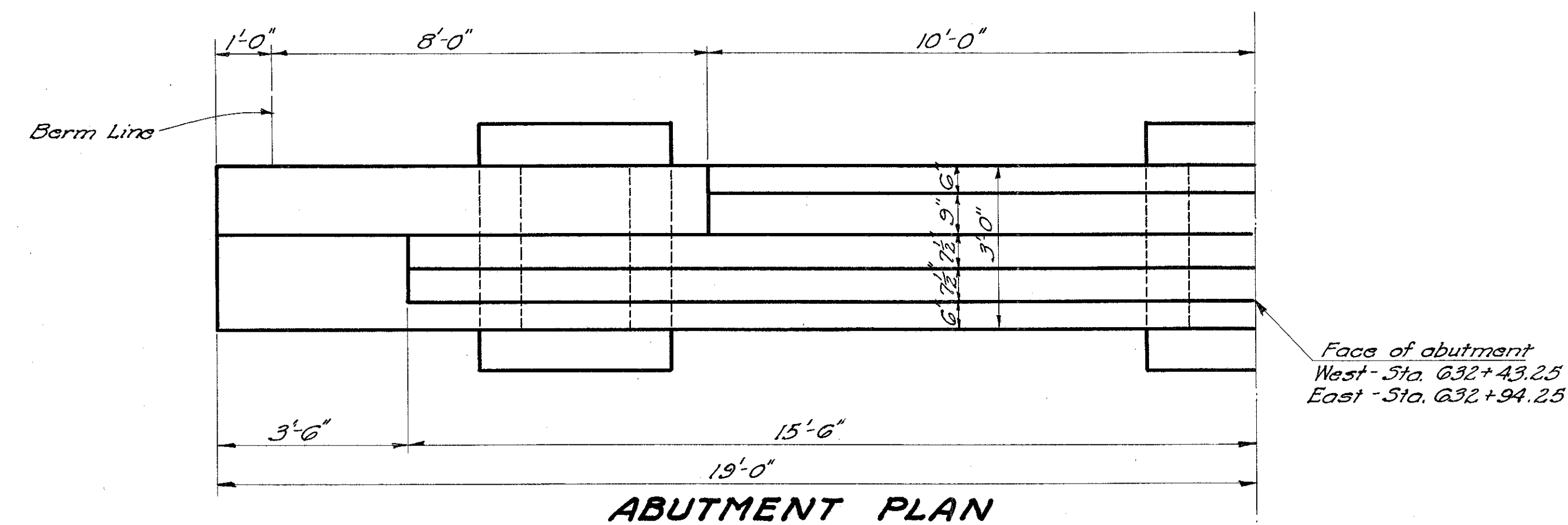
SURFACE FINISH OF CONCRETE: Fascia of deck slab shall receive a rubbed surface finish. All other exposed surfaces shall be governed the provisions of Item 5-1.

POROUS DRAINS, extending from face of abutment to Elev. 675; shall be placed on and flush with embankment slopes at all four corners of bridge. The drains shall be four feet wide and one foot thick, and shall be centered under edge of deck. They shall be composed of No. 1 or No. 12 gravel, stone, or slag. Construction procedure shall conform essentially to Item I-9. Trench excavation shall be included for payment with the price per cu.yd. bid for "Porous drains on embankment slopes."

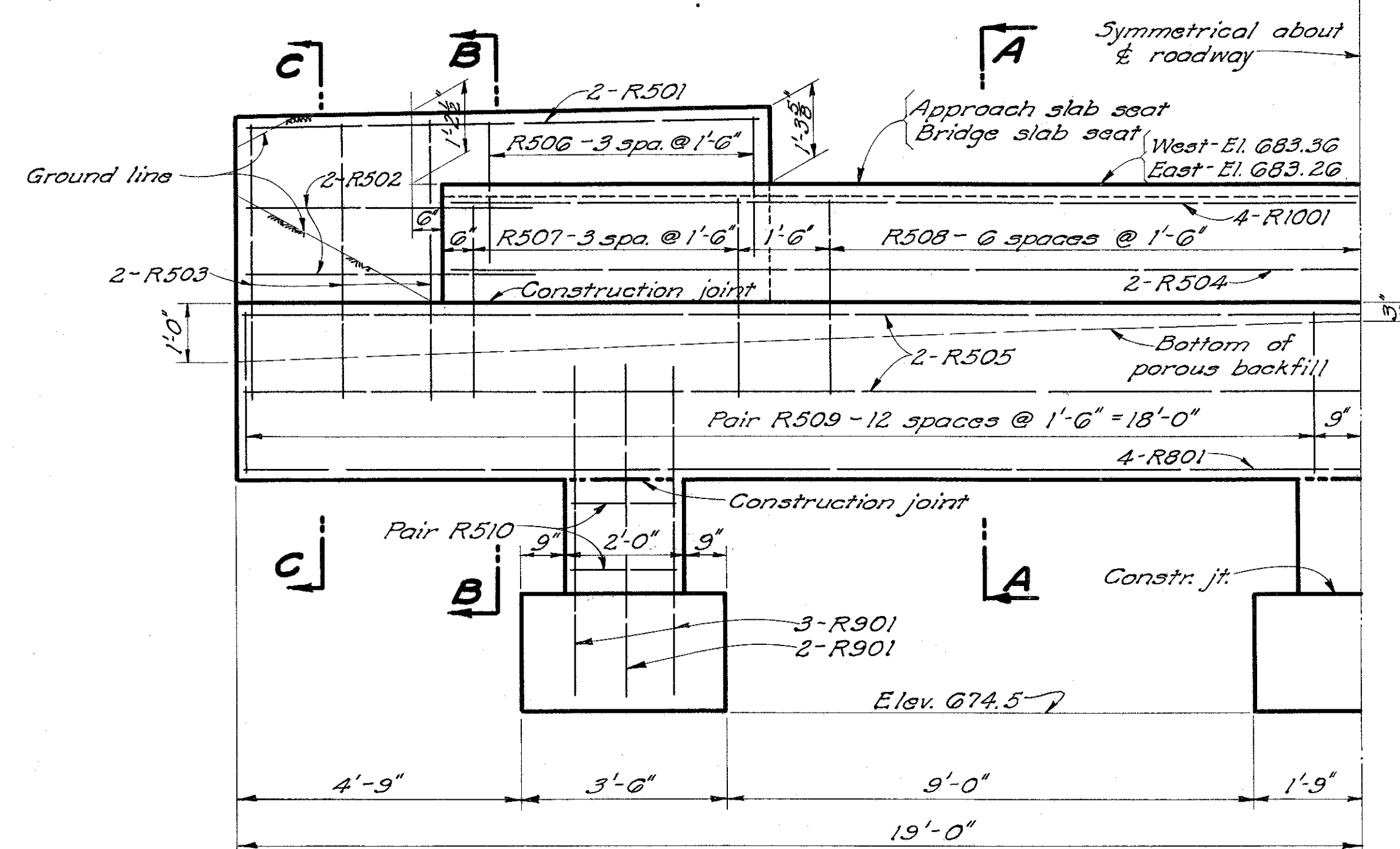
GRAVEL, if used for the coarse aggregate, shall be according to Sec. M-3.93 instead of M-3.91 for the Class "C" concrete, superstructure and pier caps. Gravel meeting the requirements of Sec. M-3.93 also may be used for other concrete in this structure.

STATE OF OHIO DEPARTMENT OF HIGHWAYS DIVISION OF DESIGN AND CONSTRUCTION BUREAU OF BRIDGES						
GENERAL PLAN ELEVATION NOTES, ESTIMATED QUANTITIES & REINFORCING STEEL LIST						
BRIDGE No. WO-281-120 over CREPS DITCH						
WOOD CO.			Sta. 632+42.00			
SEC. WO-281-(11.95)(12.88)			Sta. 632+95.50			
DESIGNED	DRAWN	TRACED	CHECKED	REVIEWED	DATE	REVISED
R.G.	R.G.	R.H.D.	cpd	q.g.	12-23-54	

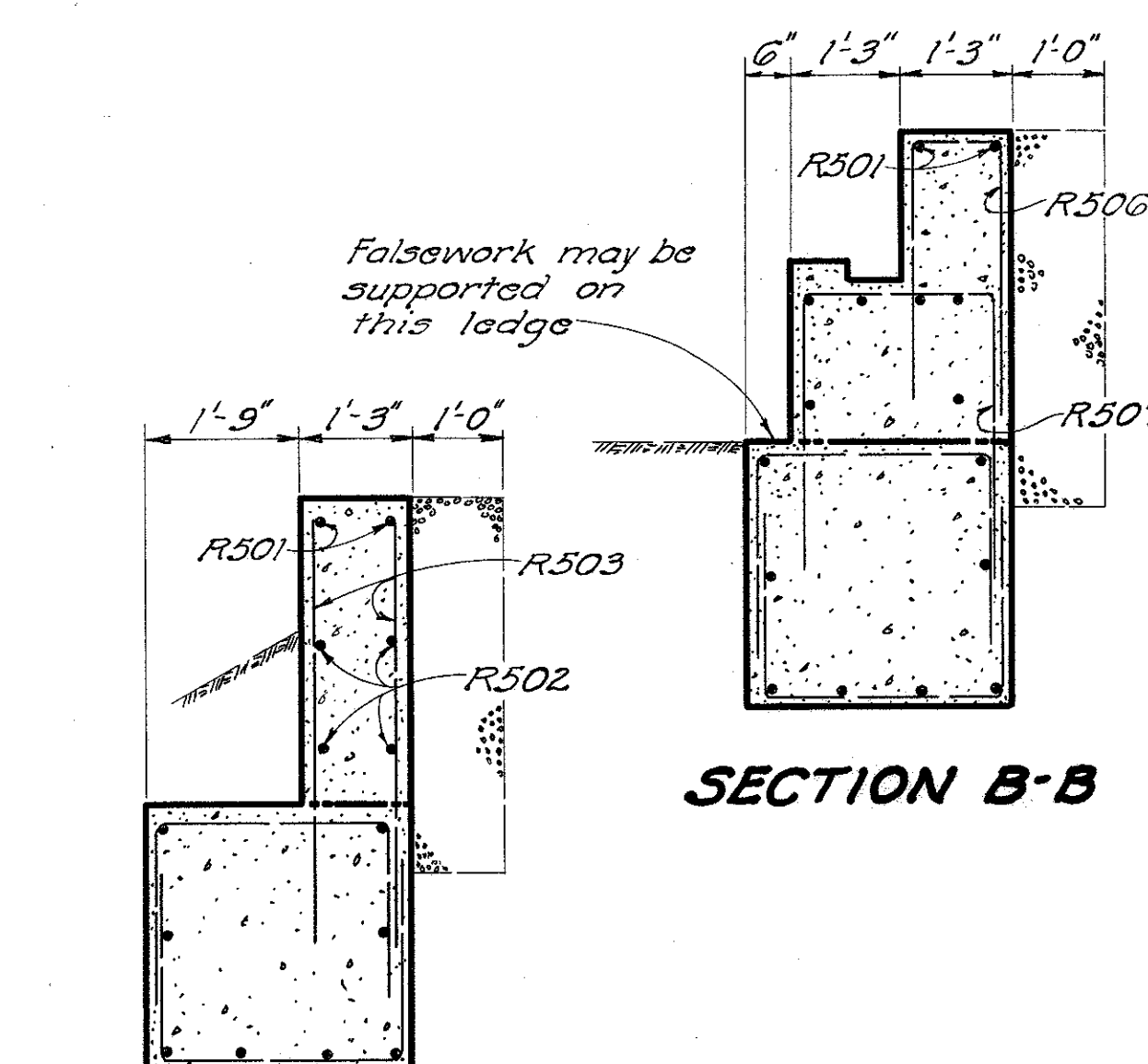
Woo-281-(11.95)(12.88)



ABUTMENT PLAN

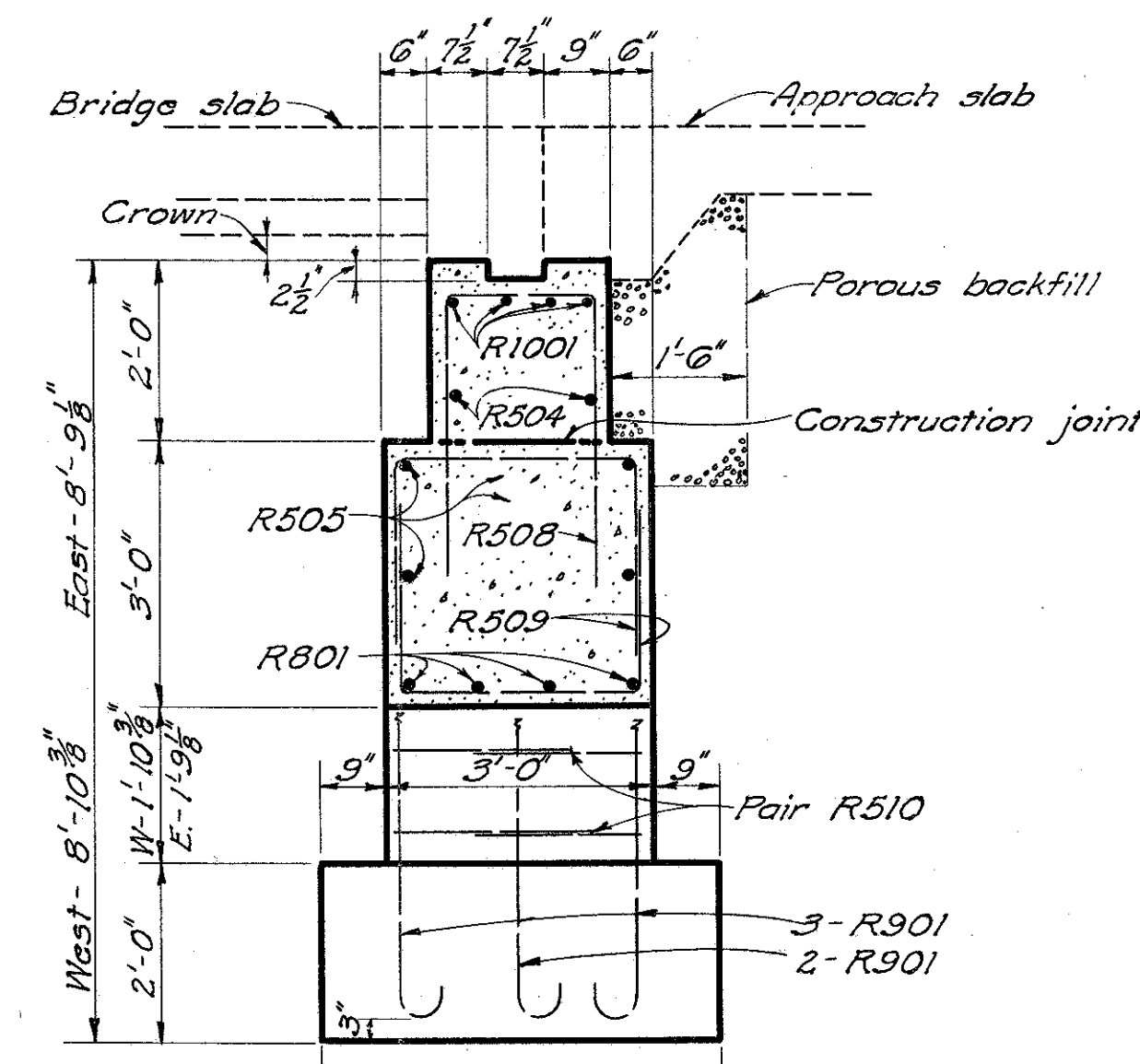


ABUTMENT ELEVATION



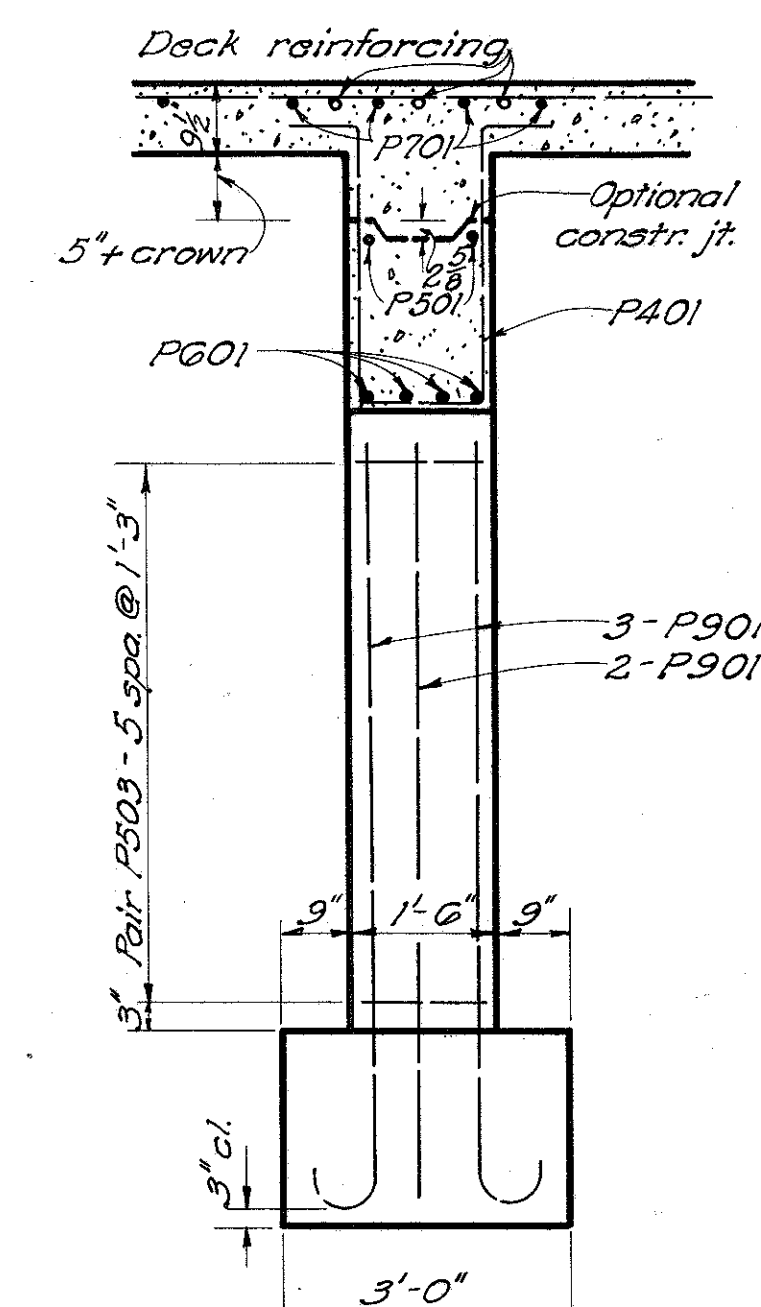
SECTION B-B

SECTION C-C

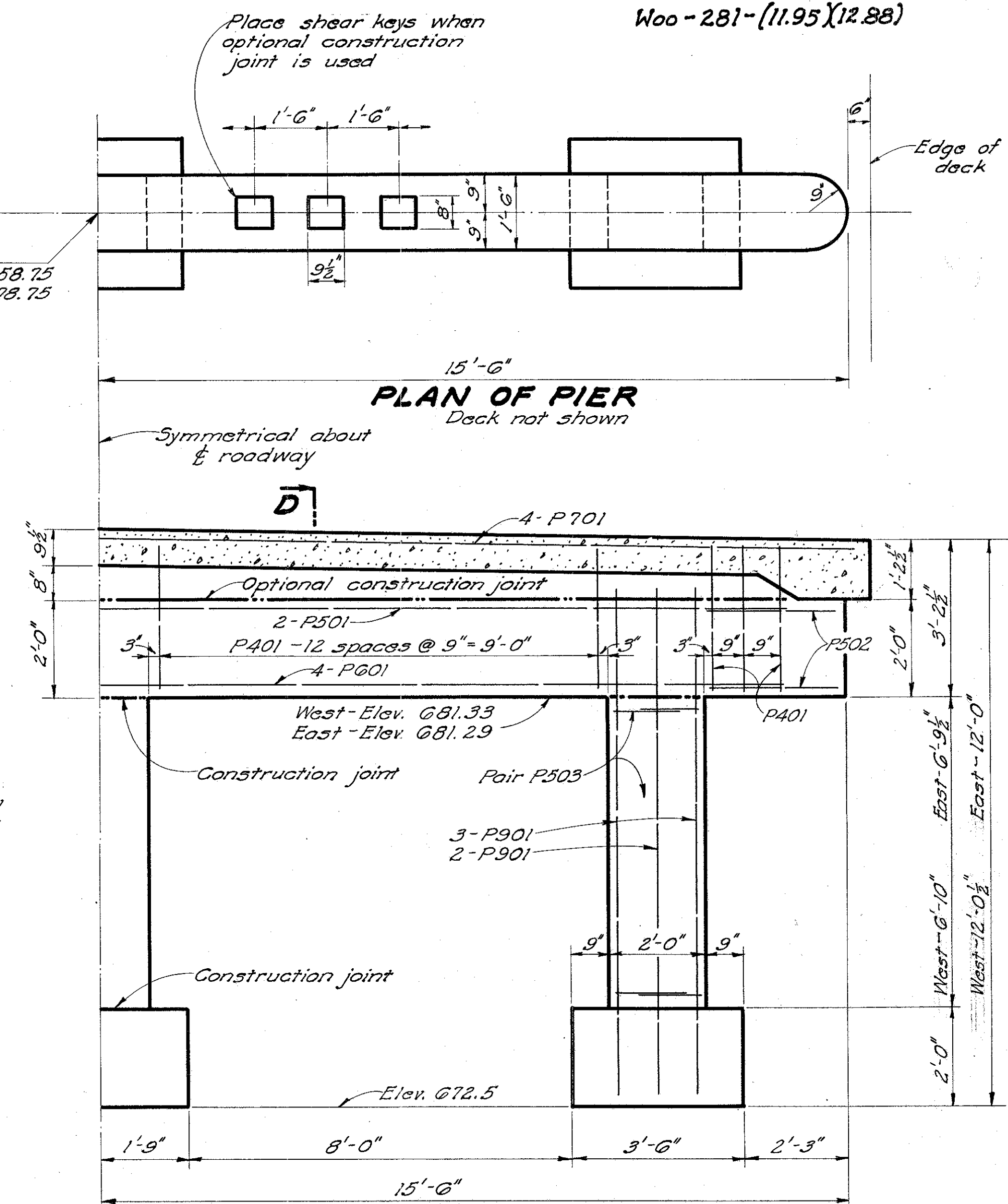


SECTION A-A

The clearance from the face of concrete to the reinforcing steel shall be 2" unless otherwise shown



SECTION D-D



PLAN OF PIER

PIER ELEVATION

ABUTMENT NOTES

CONCRETE shall be Class "E" and payment will be made on this basis, but Class "C" concrete may be used for any or all parts of the abutments.

POROUS BACKFILL shall extend upward to the approach slab and to the surface of the earth shoulders, and outward to the surface of the embankment slopes. Excavation therefor, in excess of that required for construction of the abutment, shall be considered as paid for in the bid price per cu. yd. for porous backfill.

PIER NOTES

CONCRETE shall be Class "C" for the pier cap and Class "E" for the pier columns and footings. Class "C" concrete may be used for the columns and footings, but payment will be on the basis of Class "E".

HORIZONTAL CONSTRUCTION JOINT between the top of pier cap and bottom of slab will be permitted if keys, as shown, are provided at the top of the cap. If such a joint is not provided, the concrete in the slab and cap shall be placed in a continuous operation.

PIER CAP CONCRETE QUANTITY is included in the quantity of superstructure concrete for payment.

STATE OF OHIO DEPARTMENT OF HIGHWAYS DIVISION OF DESIGN AND CONSTRUCTION BUREAU OF BRIDGES							
ABUTMENT & PIER DETAILS							
BRIDGE NO. WO-281-120 OVER CREPS DITCH							
WOOD COUNTY STA. 632+42.00 SEC. WO-281-(11.95)(12.88) 632+95.50							
DESIGNED	DRAWN	TRACED	CHECKED	REVIEWED	DATE	REVISED	
R.G.	R.G.	J.D.J.	C.P.D.	B.F.G.	12-23-54		